

The Western Edge Redevelopment Plan

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for the City of Hoboken, New Jersey

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Introduction

Statutory Basis for the Redevelopment Plan

This Redevelopment Plan has been prepared for the Western Edge Redevelopment Area within the City of Hoboken, Hudson County, New Jersey. It incorporates four properties located in the western portion of the municipality, within an area bounded by the Hudson-Bergen Light Rail (HBLR) right-of-way and the Palisades to the west; Ninth Street and a residential area to the south; Monroe Street, Madison Street and Jefferson Street and neighboring commercial and residential areas to the east; and the Fourteenth Street and Fourteenth Street viaduct and an industrial area to the north. In this plan, the Western Edge Redevelopment Area will generally be referred to as the “Redevelopment Area.” Its location is shown on **Figure 1: Location of Redevelopment Area.**

The City of Hoboken Council directed the City’s Planning Board to study the area in order to determine whether it was an “Area in Need of Redevelopment” in accordance with the criteria specified at N.J.S.A. 40A:12A-5. The City retained Phillips Preiss Shapiro Associates to conduct a Redevelopment Area investigation, which was completed in November 2006. A public hearing on the investigation was then held by the Planning Board, which recommended that the study area qualified as an **Area in Need of Redevelopment.** Based on the findings of the report, adoption of the delineated Redevelopment Area boundaries was effectuated by the City Council in July 2007.

Plan Purpose and Structure

The Western Edge Redevelopment Plan provides a framework for the redevelopment of a number of properties in the northwestern portion of the City in proximity to the Ninth Street light rail station. The Redevelopment Plan sets forth standards and guidelines for land use, circulation, open space,

parking, and urban design. Some Plan elements are fixed while other standards are flexible and are to be used as a guideline for creating development proposals that meet the Plan's goals and objectives.

Note on Plan Terminology

Throughout this Redevelopment Plan, a conscious distinction is made in the regulations between "shall" and "should."

- "Shall" or "must" means that a developer is required to comply with the specific regulation, without deviation.
- "Should" means that a developer is encouraged to comply but is not required to do so. If the exact recommendation cannot be met, the Planning Board will entertain any modification that meets the underlying spirit and intent of the regulation and/or the Redevelopment Plan generally.

Location of Redevelopment Area

As described, the Redevelopment Area encompasses a number of properties located in the western portion of the City of Hoboken near its boundaries with the City of Union City and the City of Jersey City. The City of Hoboken is located in the northeastern corner of Hudson County, New Jersey and is approximately 1.3 square miles in area. Over the last decade, the western portion of the City has evolved from an industrial, manufacturing-oriented area to a primarily residential community with some remnant industrial development.

The Redevelopment Area is located at the western edge of Hoboken, bordering the Palisades. The study area boundaries are formed by the following: the Hudson-Bergen Light Rail (HBLR) right-of-way to the west; Ninth Street to the south; Monroe Street, Madison Street and Jefferson Street to the east; and Fourteenth Street and the Fourteenth Street viaduct to the north.

Redevelopment Area Properties

Figure 2: Redevelopment Area Properties, shows the individual properties within the Plan Area.

The Redevelopment Area consists of ±11.0 acres on properties identified as 900-912 Monroe Street, 914-930 Monroe Street, 1200-1318 Monroe Street, and 1300-1330 Jefferson Street. Together, these parcels comprise a total of 143 tax lots on portions of seven tax blocks: Blocks 92, 93, 97, 101, 105, 106, 111 and 112 on the official City of Hoboken tax maps. A summary is provided below.

- **900-912 Monroe Street-** Block 92, Lots 1.1 and 1.11; Block 93, Lots 1.11, 1 to 8.1, and 25.2 to 32 comprise a ± 1.179 acre former industrial property. At the time of the Area in Need of Redevelopment Report, a vacant industrial building occupied the site. Since then, the industrial structure has been demolished in preparation for a new residential building. The proposed project known as “900 Monroe” is to include a 12 story (117 feet), 112 unit residential building with ground floor retail amounting to approximately 7,600 gross square feet and three levels of structured parking. A hardscaped public walkway along the Hudson-Bergen Light Rail (HBLR) tracks will be constructed and open to the public. The Floor Area Ratio (FAR) of the proposed project is approximately 3.0.
- **914-930 Monroe Street-** Block 92, Lots 1.2 and 1.12; Block 93, Lots 1.12 and 8.2 to 25.1; Block 97, Lots 1.1 to 22.2; Block 101, Lot 1.1 and Block 105, Lot 1.1 comprise an industrial property with a older structure that is used for warehousing and distribution purposes. The site totals ± 4.42 acres and is irregular in shape.
- **1200-1318 Monroe Street-** Block 106, Lots 1 to 32 and Block 111, Lots 2 to 14 comprise a former industrial property with frontage on Monroe and Twelfth Streets, extending north to Fourteenth Street. The parcel totals ± 3.51 acres. The property is improved with a one-to two-story concrete building that is currently vacant.
- **1300-1330 Jefferson Street-** Block 112, Lots 1 to 32 comprise a total of ± 1.83 acres that is currently improved with a vacant one-story metal-clad building.

Land Uses and Locational Context

The Redevelopment Area is surrounded by a mix of uses, including high-rise multi-family residential to the south, the Hudson-Bergen Light Rail (HBLR) right-of-way to the west (marking the municipal boundary), industrial uses to the north, and public, industrial, commercial, and multi-family residential uses to the east. The remainder of the surrounding area is largely multi-family residential in nature. Land uses adjoining the Redevelopment Area are shown on **Figure 3: Land Uses in the Vicinity of Redevelopment Area Properties**, and **Figure 4: Site Context Aerial Photograph**.

Although the Plan Area is on the western municipal boundary, it is well integrated into the surrounding street grid network. All of the roadways radiating from the Redevelopment Area connect to the core of the City of Hoboken. However, the Area’s location on the westernmost edge of the City bordered by the Palisades cliffs makes it entirely inaccessible from the west. There are

three east-west streets that terminate at the eastern edge of the Redevelopment Area: Thirteenth Street, Twelfth Street, and Tenth Street. There are also four north-south streets that provide access: Jackson Street, Jefferson Street, Monroe Street, and Madison Street. Jackson Street terminates at the southwesterly edge of the Redevelopment Area and Monroe Street follows the eastern edge of 900-912 and 914-930 Monroe Street and then terminates at its intersection with Twelfth Street.

South and east of the Redevelopment Area are several multi-family residential buildings that have been recently constructed in connection with the Northwest Redevelopment Plan, as well as commercial development that includes a ShopRite grocery store. The Northwest Redevelopment Plan Area wraps around the study area to the east and south and consists of all or parts of 22 tax blocks, of which four directly abut study area properties on Ninth Street, Monroe Street, and Jefferson Street.

To the north, the Western Edge Redevelopment Area is bounded by an elevated roadway known as the Fourteenth Street Viaduct. Further to the north is an active industrial and warehousing area, a large part of which is occupied by Academy Bus, which is essentially cut off from the Redevelopment Area properties south of Fourteenth Street by the viaduct infrastructure.

To the west lies the municipal boundary with Union City and the right of way for the Hudson-Bergen Light Rail. The light rail runs along the western boundary of the study area. The Ninth Street light rail station is located at the westernmost end of Ninth Street, which is at the southwesterly edge of the Redevelopment Area.

While the City of Hoboken has experienced a development boom in the last two decades, the northwestern portion of the City is one of the few areas of Hoboken that still contains parcels which are remnants of its industrial past. However, the area has also experienced heightened construction activity in recent years, especially in response to the Northwest Redevelopment Plan. Most of the new construction has been either residential or commercial, providing new housing options and choices within the City. The development that has occurred in response to the Northwest Redevelopment Plan represents the largest new residential development that has been built in the area in recent years.



Figure 1: Location of Redevelopment Area

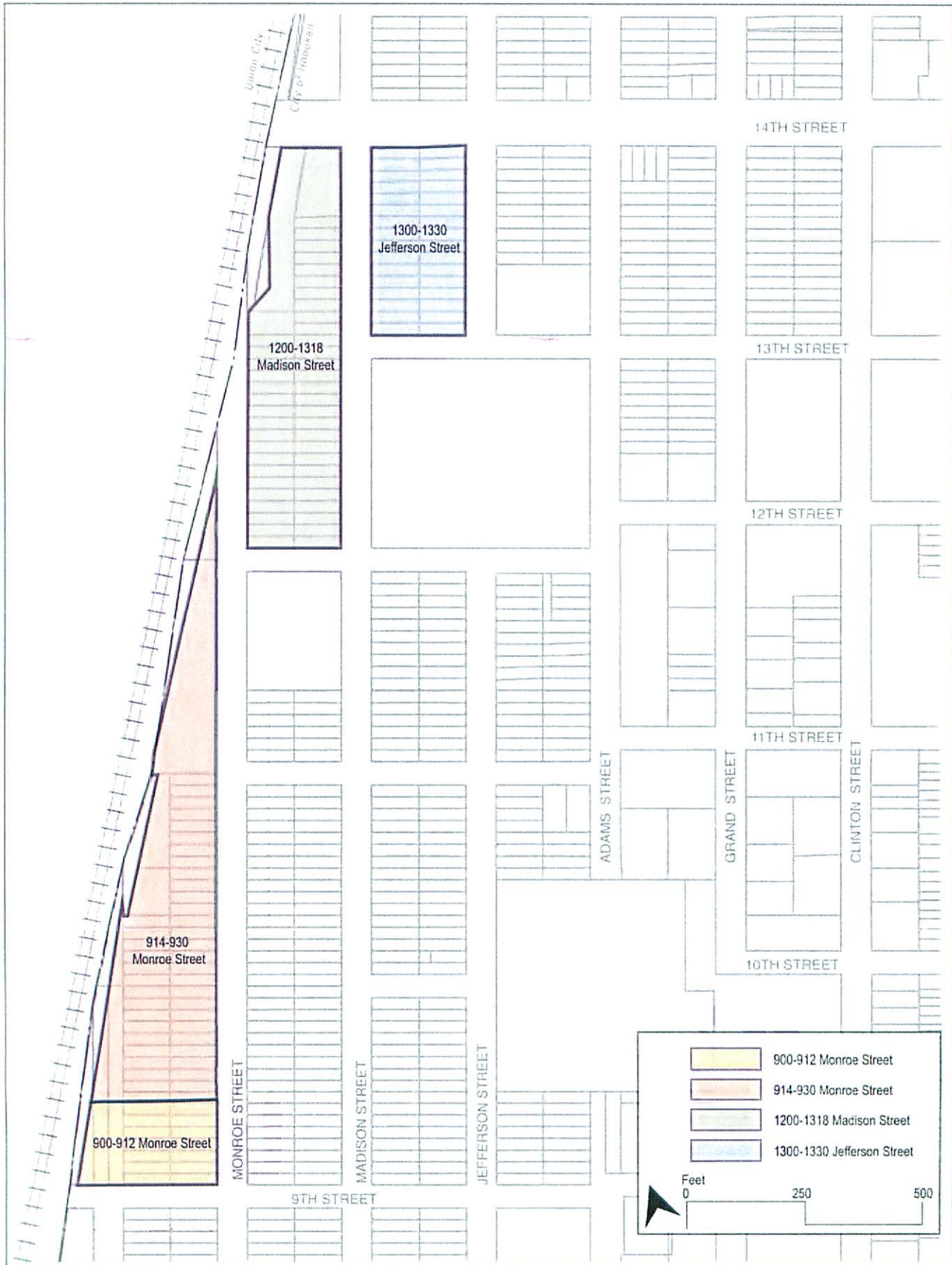


Figure 2: Redevelopment Area Properties

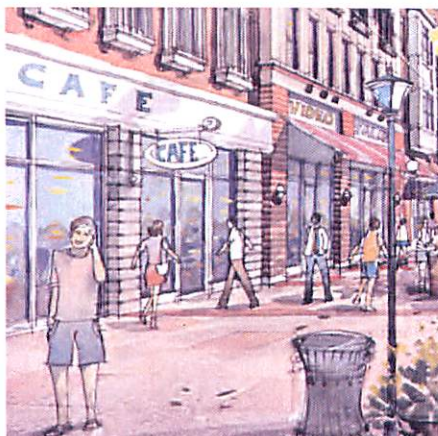


Figure 3: Land Uses in the Vicinity of Redevelopment Area Properties



Figure 4: Site Context Aerial Photograph

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Vision, Goals, & Context

Vision

The Western Edge Redevelopment Area is located within what the City's Master Plan refers to as Hoboken's final frontier; one of the last vestiges of its industrial past where mostly vacant or underutilized industrial/manufacturing facilities remain. Yet, this roughly 11-acre area next to the Palisades cliffs and the Hudson-Bergen Light Rail line presents a tremendous opportunity for the City to achieve many of the goals and objectives articulated in the Master Plan, perhaps none more important than the recognized need for additional parks, recreation and open space. The need for additional retail uses and services to satisfy the demands of a growing residential population in northwest Hoboken has also become apparent. Likewise, the City recognizes that it must continue to seek out opportunities by which it can provide affordable housing for its residents. This Plan strives to realize all of these goals by combining public open spaces and recreation; pedestrian-oriented retail services; affordable housing; an attractive streetscape; and well designed residential buildings in the form of an integrated, mixed-use redevelopment program.

Redevelopment Plan Goals and Objectives

The following is a summary of the key Redevelopment Plan goals and objectives.

- Provide for an increase in the economic base of the Redevelopment Area and the entire City by redeveloping underutilized and non-productive properties.
- Provide for the revitalization of an area with an unpleasant and uninviting streetscape.
- Provide for a significant amount of open space and recreation opportunities that are accessible to the residents of the City of Hoboken.
- Provide for expanded retail floor space to serve the needs of the growing resident population in the northwestern portion of the City.
- Provide for landscaped rooftops/decks or green roofs that can benefit the

environment and serve as an open space amenity for project residents.

- Create a portion of a multi-use recreation/walking path or green circuit around the City adjacent to the light rail tracks.
- Take advantage of the light rail by encouraging higher-density residential, open space, and pedestrian-friendly uses around the transit station consistent with smart growth planning principles.
- Provide for a variety of housing types, including both market-rate and affordable housing which complements and expands the housing options and choices in the City.
- Allow taller buildings in exchange for public open space while preserving view corridors to the Palisades.
- Create a walkable, human-scaled community that integrates new development with surrounding residential areas and provides connections to the 9th Street Hudson-Bergen Light Rail (HBLR) station, including greenways and pedestrian linkages.
- Establish more varied site and building design standards that foster a visually pleasing streetscape and high-quality building design within the Redevelopment Area.

Relationship to Intent and Purpose of the Master Plan

New Jersey's redevelopment statute requires that "all provisions of the Redevelopment Plan shall be either substantially consistent with the municipal master plan or designed to effectuate the master plan." The Western Edge Redevelopment Plan is written to incorporate the major goals, objectives and concepts of the Master Plan. The Plan is substantially consistent with and designed to effectuate the City's Master Plan, the last comprehensive version of which was adopted in April 2004.

Specifically, this Redevelopment Plan complies with the intent of the Master Plan with respect to the following general goals and objectives:

- Amplify Hoboken's sense of community, encompassing its social diversity.
- Enhance Hoboken's unique setting as an urban enclave facing New York Harbor.
- Improve the appearance of Hoboken's streets.
- Enhance its walkability and pedestrian amenities.
- Contemporize its community facilities.
- Provide additional open space and recreation facilities.

The Redevelopment Plan supports a number of the recommendations contained in the Master Plan, including those listed below.

Land Use General Concepts

- Promote and enhance Hoboken's historic character and design image.

- Continue to promote a pedestrian-friendly environment.
- Maintain an appropriate mix of uses.
- Enhance physical and visual connections between the waterfront and the rest of the City; and between the Palisades and City.
- Coordinate development decisions with adjacent municipalities and Hudson County.
- Work with institutions to ensure that any future growth is appropriate in terms of location, scale and design.

Building and Site Design

- Promote compatibility in scale, density, design, and orientation between new and existing development.
- Require buildings to be oriented to the street.
- Continue to promote stoops (and stoop life).
- Continue to hide parking on the ground level of buildings.
- Enact “green architecture” requirements for new construction.
- Enact “quality housing” model design guidelines for new construction.

Streetscape Design

- Provide additional street trees.
- Prohibit new surface parking lots or other open parking areas.
- Restrict new curb cuts.

West Side/Northwest Redevelopment Area

- Provide additional open space and community facilities as other development occurs. To the greatest extent possible, new development should contribute to the provision of these elements.
- Improve the area underneath the Fourteenth Street Viaduct. The area underneath the viaduct has the potential to serve as open space and/or a unifying feature for this section of the City.

Open Space, Recreation and Conservation

- Create a green circuit in the City to link recreational and other amenities (creating a multi-use path around Hoboken’s periphery).
- Maximize park and recreation opportunities for residents.
- Involve the private sector in creating open space.
- Create park corridors or “green streets.”
- Require street trees as part of development applications.
- Provide more recreation and parks through better utilization of land.

Community Facilities

- Encourage environmentally sensitive and sustainable design.
- Promote the improvement of utility systems in the City.
- Promote the creation of green roofs and parking lots.

- Employ Quality Housing Zoning to promote even higher standards.
- Address drainage problems in the City's flood zones.
- Develop a set of stormwater management policies and regulations for new development.
- Reduce waste and promote recycling.

Circulation and Parking

- Enhance walkability throughout the City.
- Provide adequate lighting of sidewalks.
- Require bicycle storage facilities in new development.
- Create a multi-use circuit around the City, if possible adjacent to the light rail tracks.

Economic Development

- Encourage a mix of uses in new developments to provide supporting services to workers and residents.
- Promote convenience retail at the new light rail transit stops.
- Create opportunities for more gathering places.

Housing

- Provide diversity in types of housing.
- Provide additional affordable units in new residential developments.
- Create a Quality Housing model for new or rehabilitated housing.

Historic Preservation

- Encourage contemporary building designs for new construction that complement Hoboken's historic buildings without mimicking them.

Specific Master Plan Recommendations for Redevelopment Area

The Redevelopment Area is included on several concept plans within the Master Plan. The *Open Space Concept Plan* identifies two of the Redevelopment Area parcels for planned or possible new parks and recreation: **900-912 Monroe Street** and **914-930 Monroe Street**. The *Economic Development Plan* places the majority of these two blocks in the "Transit-Oriented Business District" which are districts located adjacent to light rail stations which are described as "appropriate locations for commercial development" and "logical places for small business districts to be created." On the *Northwest Area Concept Plan* the Master Plan designates **1200-1318 Monroe Street** as "Residential with Ground Floor Commercial" uses and **1300-1330 Jefferson Street** is designated as "Existing/Possible Public School." The language which accompanies the *Northwest Area Concept Plan* states that Hoboken is in need of additional open space and community facilities and recommends that new development contribute to the creation of these elements in order to help create a more balanced neighborhood context.

As indicated on the Land Use Plan, the Redevelopment Area is entirely located within the *IT Industrial Transition zone*. This is depicted in **Figure 5: Land Use Plan from City of Hoboken Master Plan, 2004**. By designating the area in the Industrial Transition (IT) zone on the future land use map, the Master Plan recognized that this portion of Hoboken was in the process of transformation. Thus, while the Plan states that limited industrial uses will continue to be permitted in these areas, it recommends that alternative uses be encouraged, including public facilities, office development and residences. In particular, the Plan recommends that residential uses “be permitted only as a conditional uses in accordance with specific requirements.” These requirements include being located adjacent to public parks, or by providing open space that is dedicated to the City. Building heights and densities could also be increased in return for the provision of public amenities.

As this Redevelopment Plan provides for planned residential development with significant public open space, it is substantially consistent with and designed to effectuate the Land Use Plan Element of the City’s 2004 Master Plan.

Relationship to Intent and Purpose of the Zoning Code

Under the current zone plan, the Redevelopment Area lies entirely within the *I-1 Industrial* zoning district. Zoning boundaries are shown in **Figure 6: Redevelopment Area Properties and Adjacent Zoning**. The I-1 zone encompasses an eight block long area extending north-south in an area east of the Hudson-Bergen Light Rail (HBLR) right-of-way. Immediately to the east and south the I-1 zone is bounded by the Northwest Redevelopment Plan (NWRDV) area, which is mapped over portions of the former I-1 zone.

Permitted uses in the *I-1 Industrial* zone include manufacturing, processing and fabricating operations, warehouses, office buildings and research laboratories, and public services. Conditional uses include automobile service stations, garages, building supply and lumber yards, factory outlet stores, planned industrial development and parking.

Bulk standards in the *I-1 Industrial* zone require a minimum lot area of 20,000 square feet, minimum lot width of 200 feet and minimum lot depth of 100 feet. The zone has a required minimum front yard setback of 10 feet, a minimum side yard setback of 10 feet, and a minimum rear yard setback of 20 feet. The maximum building height is four stories or 80 feet for principal buildings and one-and-one-half stories or 30 feet for accessory buildings, and the maximum lot coverage is 65 percent for principal buildings and 10 percent for accessory buildings. ment for the Redevelopment Area over the long term.

The vision for mixed-use development in the Redevelopment Area is not reflected in the Zoning Ordinance. As such, the zoning is not consistent with the use and bulk regulations contained within this Plan. The most recent Master Plan has recommended land use changes which do not support industrial development for the Redevelopment Area over the long term.

City of Hoboken

MASTER PLAN

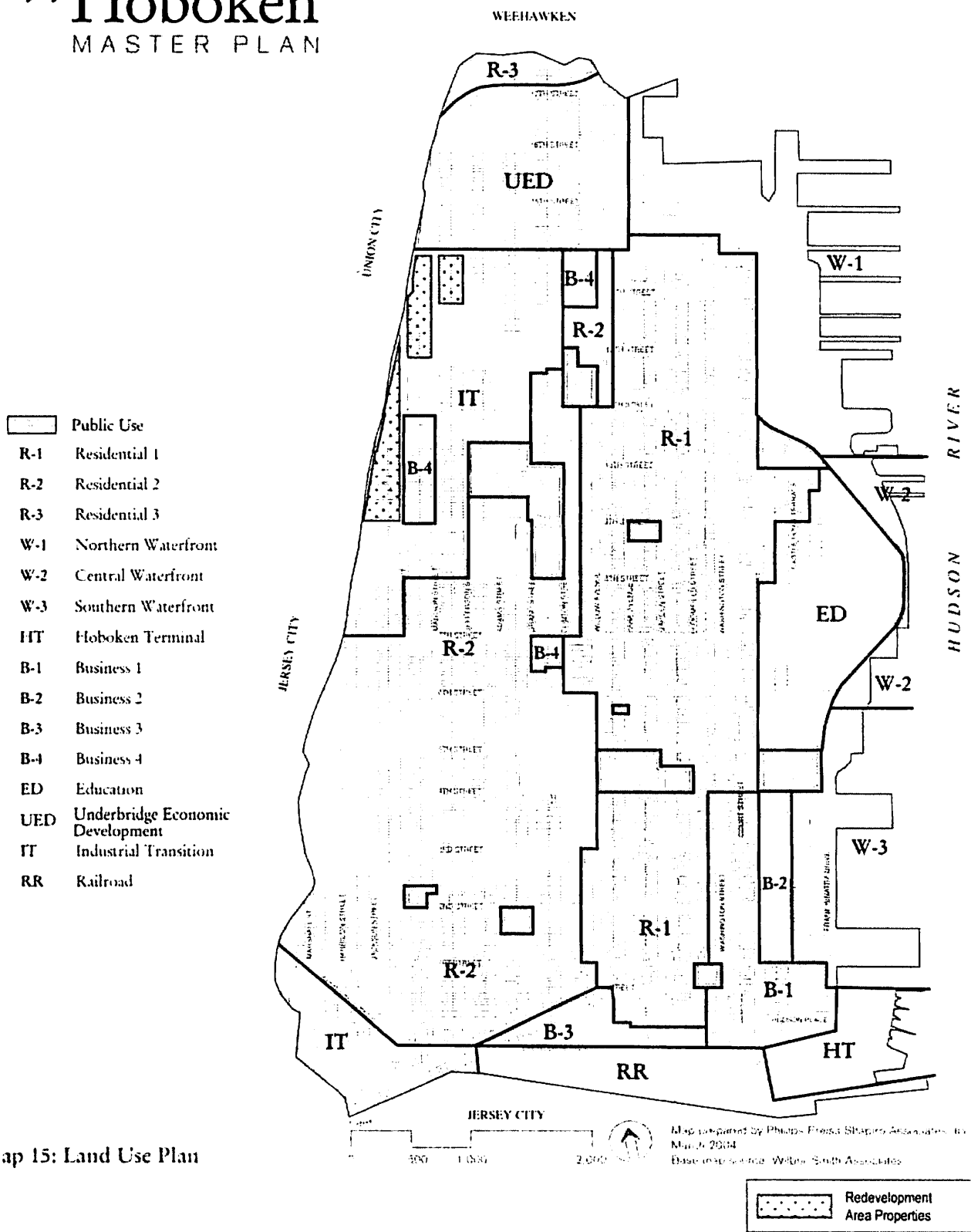


Figure 5: Land Use Plan from the City of Hoboken Master Plan, 2004

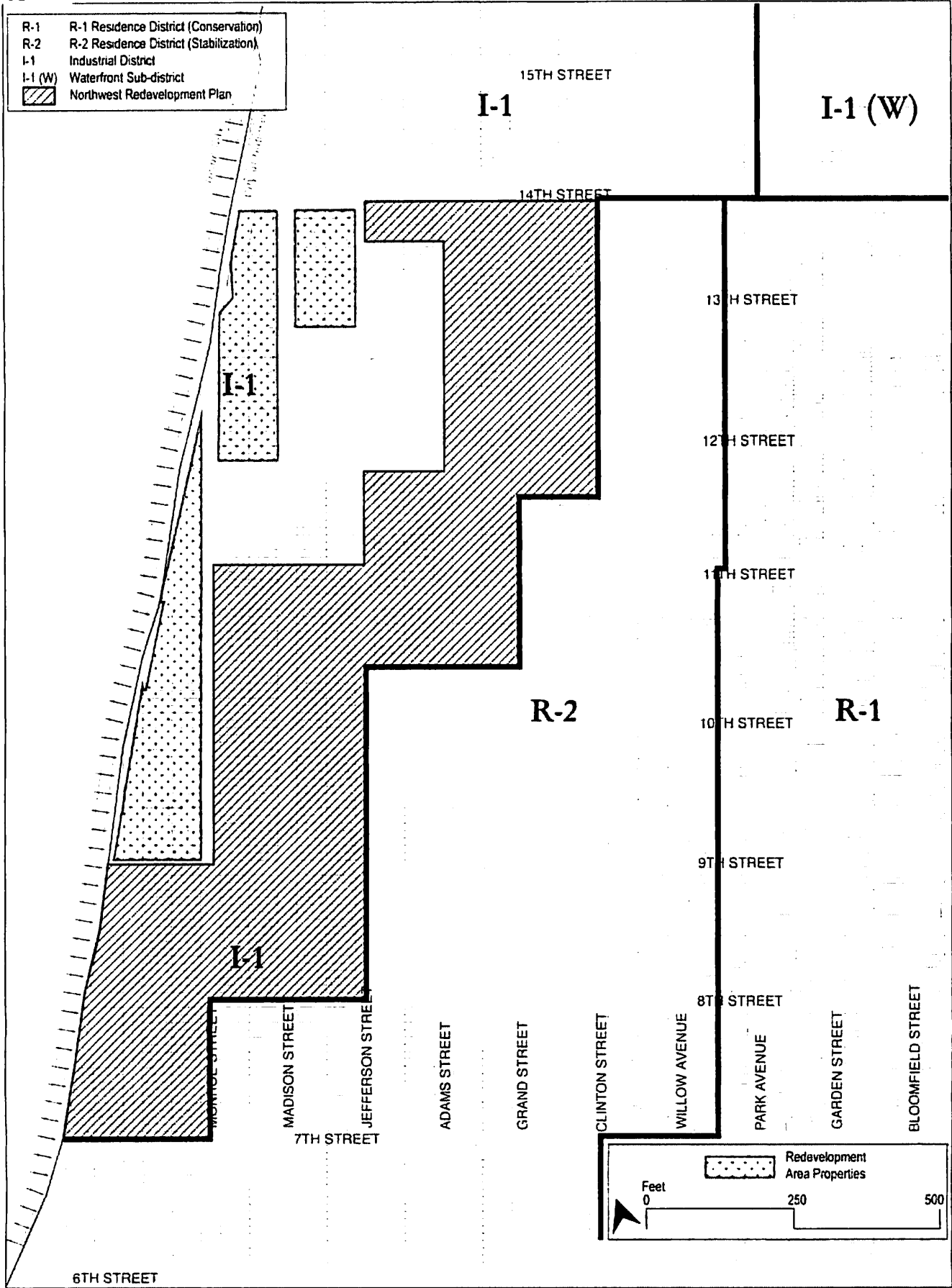


Figure 6: Redevelopment Area Properties and Adjacent Zoning

3



Public Realm: Streetscape, Open Space and Recreation

Intent

This section sets forth standards that seek to create a pedestrian-friendly environment and attractive streetscape, and to further provide appropriate open space and recreational facilities for the residents of the City of Hoboken.

Streets, parks, walkways, and other public spaces constitute the “public realm,” or the primary public spaces that form the character and identity of the Redevelopment Area. They provide the physical framework within which new building construction will occur and create the kind of environment that residents, shoppers, workers and visitors will experience.

Streets

The Redevelopment Area can be reached along the existing street grid of northwest Hoboken including the north-south roads of Monroe, Madison and Jefferson Streets, and the existing east-west roads of 9th, 11th, 12th, 13th and 14th Streets. The goal is to improve the pedestrian experience along these roads and to introduce street trees, greenery, sidewalks, and human-scale building forms along the perimeter of the Plan Area. Another goal is to integrate the grid pattern into the designs for the open space areas by providing an open space focal point where selected east-west streets meet the open space within the Redevelopment Area.

Streetscape Improvements

- All existing above-ground utility wires and facilities shall be relocated underground, and any new utility wires shall be located underground.
- Where existing trees do not exist, street trees shall be planted, at approximately 30 to 40 feet on center, along the curbside edge of the public sidewalk on the streets. Exceptions to the 30- to 40-foot spacing area are allowed for curb cuts to parking areas and lobby entrances.

- Public benches shall be provided along Monroe, Madison and Jefferson Streets, especially near the parks and other open space.
- Bicycle racks shall be provided outdoors at each shared building entry, within 20 feet of building entrances. Bicycle racks may be located on public streets or within building setbacks.

Open Space and Recreation

The Redevelopment Plan seeks to provide a significant amount of open space and recreation for residents of the City of Hoboken. Open space may include playgrounds, tot lots, grassed sitting areas, lawns, gardens, dog runs, jogging/walking paths and seating areas with benches/kiosks, etc. The Plan further requires development of a community center with an outdoor public pool facility that may be developed on- or off-site.

The Plan requires the following types of open spaces be provided:

- At grade, publicly accessible open space (parks, playgrounds, plazas, trails, green circuit, etc.) shall occupy at least forty (40) percent of the total area of the Redevelopment Area, i.e. ±4.4 acres, excluding public streets.
- A community center and outdoor public pool shall be provided on- or off-site.

Passive Parks

Two areas within the Redevelopment Plan shall be reserved for use as park space. Refer to **Figure 7: Conceptual Plan**.

Park Block 1

- Park Block 1 is bounded by Monroe Street to the east and the HBLR right-of-way to the west. The area extends west of Monroe Street from 11th Street to 12th Streets and is ±1.4 acres in area.

Park Block 2

- Park Block 2 is located at the northwest corner of Monroe and Thirteenth Streets and extends along Monroe Street north to the elevated Fourteenth Street Viaduct and is ±1.73 acres in area. The HBLR right-of-way extends along its western edge.

Design Guidelines for Park Blocks 1 and 2

- Either a mews or a circular, hardscaped plaza should be provided at the end of each of the number streets (11th, 12th, 13th Streets) to provide a sense of arrival to the open space, as well as to provide a visual terminus along those streets.
- Parks should be located at the grade of the sidewalk, with barrier-free access.
- There shall be no buildings on either Park Block 1 or 2.
- There shall be a variety of seating areas provided throughout the park blocks.

1 Mews, pedestrian-only public rights-of-ways. Walkways shall be paved and may include additional plantings or hardscape. They provide access to the front door of units, but they do not provide vehicular travel, surface parking, or access to residential garages. They are typically connected to perimeter streets or open spaces at both ends, and complemented by an alley that provides access to parking at the rear of the units. The front door of all units on a walkway should be no more than 200 feet from a vehicular street. The public right-of-way of walkways shall be at least 25 feet in width to ensure sufficient light and air and visibility for safety,



Figure 7: Conceptual Plan



Figure 8: Looking Northeast Along the Green Circuit

- Landscaping of parks, playgrounds, plazas, trails and other public areas may include public gardens, trees, shrubs, ground cover, planters and flowers, as well as water features, sculptures, gazebos, kiosks, art and similar materials, and shall be designed to fulfill aesthetic environmental, ornamental and related objectives.
- There shall be at least one dog run and one tot lot on the Park Blocks.

Green Circuit

A portion of the “green circuit” shall be built along the HBLR right-of-way in the Redevelopment Area as per the 2004 *Hoboken Master Plan*. The green circuit should be no less than 25 feet in width at any point and should include a landscaped buffer of between three (3) and five (5) feet abutting the HBLR tracks and the building zones. The buffer between the green circuit and the building zones shall consist of low scale plantings to provide for a visible, safe circuit. The buffer between the green circuit and the HBLR right-of-way shall consist of trees to mask the train tracks. Refer to **Figure 8: Looking Northeast Along the Green Circuit**.

- The green circuit shall be a continuous path that runs from the southernmost end of the Plan Area at 9th Street to the northernmost end at the 14th Street viaduct.
- Benches and lighting should be provided along the green circuit.
- A portion of the green circuit shall be hardscaped so as to provide barrier-free paths and to allow cyclists.
- Public access to the green circuit shall be provided along its entire length.

Community Center and Pool

A community center and outdoor public pool shall be provided on-site or at an off-site location that is within 500 feet of the boundaries of the Redevelopment Area. The minimum site area for the community center/public pool shall be 30,000 square feet. The facility shall provide for the following amenities: a minimum two-story, 25,000 square foot building designed to accommodate gymnasium space with basketball courts; men's and women's locker rooms; arts and crafts space; art and dance studios; a computer lab; offices; and an outdoor swimming pool and kiddie pool.

4

Land Use, Height, Bulk and Parking



Building Zones

Four areas within the Redevelopment Area shall be designated as Building Zones for residential and/or mixed-use development.

900 Monroe Building Zone

- The 900 Monroe Building Zone includes: Block 92, Lots 1.1 and 1.11; and Block 93, Lots 1.11, 1 to 8.1, and 25.2 to 32. It is roughly square in shape, and is bounded on the south by Ninth Street, on the east by Monroe Street, on the west by the HBLR right-of-way and on the north by Building Zone 1. It totals ±1.179 acres.

Building Zone 1

- Building Zone 1 is largely triangular in shape, and is ±3.5 acres. It is bounded by Monroe Street to the east, the HBLR right-of-way to the west, the 900 Monroe Building Zone to the south, and a line created by a westerly extension of 11th Street to the north.

Building Zones 2

- Building Zone 2 is located north of Twelfth Street, south of Thirteenth Street, west of Madison Street and east of the HBLR right-of-way. It totals ±1.73 acres.

Building Zones 3

- Building Zone 3 is rectangular in shape and is bounded by Thirteenth Street on its south, Jefferson Street on its east, Fourteenth Street and the elevated Fourteenth Street viaduct on its north, and Madison Street on its west. It is ±1.84 acres.

Refer to Figure 7, Conceptual Plan.

Land Uses

Primary Permitted Uses in All Building Zones

The uses listed below are permitted within the four Building Zones.

Parks and open space

- Parks, pocket parks, greenways, linear parks and other passive open spaces such as dog runs, lawns, plazas, courtyards, gardens, sitting areas and playgrounds open to the public.
- Portions of the "Green circuit"

Multi-family residential units

- Multi-family residential units typically have elevators serving upper-level units; however, they may also be built as "walk-up" buildings with staircases only.
- Except as provided herein, multi-family residential units are permitted on all levels of buildings, inclusive of a full range of dwelling unit sizes from studio to three-bedrooms.

Secondary Uses Permitted Only on the First Level in All Building Zones

The following uses are permitted only on the first level of building(s). The "first level" definition and specific regulations are described in the next section.

- Retail and business services
- Restaurants and cafes
- Banking and financial institutions
- Health clubs open to the public
- Family day care
- Child care centers
- Clubs, community centers, and community meeting rooms

Permitted Accessory Uses in All Building Zones

The following uses are permitted as accessory to the primary and secondary permitted uses.

Off-street parking

- All off-street parking shall be in structured parking within buildings; surface parking and open-air rooftop parking are prohibited.
- Short-term parking spaces (15 to 30 minutes) for pick-up and drop-off are allowed outside the main entrances of the buildings.
- Any at-grade or above-grade level of parking within buildings shall be screened by primary or secondary permitted uses or by architectural detailing, as discussed further below in the section on *Parking*.
- Stand-alone parking garages are prohibited.
- All loading spaces for uses within the Plan Area must be located at the side, rear or interior of buildings, away from street-facing frontages.



Figure 9: Ground-Floor Retail on Monroe Street

“Back room” Facilities

- Facilities associated with residential buildings, such as mechanical, loading, transformers, laundry, trash rooms, storage lockers, or other service-type facilities.
- Bicycle storage rooms or areas.

Other Accessory Uses

- Any uses customarily incidental to principal or secondary permitted uses.
- Landscaped rooftop decks are permitted within all building zones.

Regulation for First Level Uses

The goal of the “first level” regulations is to maintain a relationship between buildings and the street and public open space, including the green circuit. This relationship can be achieved by providing active uses and windows

close to the sidewalk rather than blank walls or large expanses of parking. The “first level” of building and parking areas is defined as the lowest level with a finished floor elevation between zero (0) and five (5) feet above grade, measured with respect to the grade at the closest public sidewalk. The following regulations apply to the finished floor elevation of different first level uses.

- The finished floor of the first level of any residential (primary) uses shall be between two (2) and five (5) feet above grade.
- The finished floor of any non-residential (secondary) uses shall be between zero (0) and five (5) feet above grade.

Non-Residential Yield

A minimum of 25,000 square feet of retail or other permitted secondary uses shall be built on the first level along 9th Street and on Monroe Street between 9th and 11th Streets. A maximum of 90,000 square feet of retail or other permitted secondary uses is allowed on the first level along any street. Ground floor retail space shall have a minimum height of 12 feet.

Refer to **Figure 9: Ground Floor Retail on Monroe Street** for an illustration of active uses and windows facing the sidewalk.

Street Frontages

Along frontages with all public streets except 14th Street, as well as along the mews or hardscape areas at the extension of 11th, 12th and 13th Streets, first level uses shall consist only of primary and/or secondary permitted uses.

Green Circuit and Open Space Frontages

Adjacent to the green circuit and other open space areas, primary or secondary uses are recommended, but not required along the first level of buildings. Where no primary or secondary uses are provided, any off-street parking at or above grade shall be screened behind architectural detailing (as described in the Section on *Parking Screening Methods*).

Floor Area Ratio

Floor Area Ratio (FAR) is the ratio of the total floor area of buildings, excluding building areas devoted to parking and loading, to the total area of the Redevelopment Area, excluding public streets. The maximum floor area ratio shall be 3.30.

Affordable Housing and Bedroom Distribution Requirements

- Affordable housing is required. At least 100 units shall be provided on-site and such units shall be integrated within the same buildings as market-rate



Figure 10: Illustration Showing Height of 900 Monroe Building Block

units. The provision of such affordable units shall be consistent with all other applicable rules of the New Jersey Council on Affordable Housing.

- At least 5% of all market-rate units constructed within the Redevelopment Area shall have 3 bedrooms.

Building Envelope Regulations for All Building Zones

Building Height

Building height is measured according to the definition in the Hoboken zoning code². The maximum height is measured at the roof slab. The following height limits apply in the Redevelopment Area.

- Refer to **Figures 11 through 13**, which show the rationale behind the height limits for Building Zones 1, 2 and 3. The goal is to keep the overall building height below that of the top of the Palisades. Refer also to **Figure 10: Illustration Showing Approved Development within 900 Monroe Building Zone**.

900 Monroe Building Zone-	The maximum height permitted is 120 feet.
Building Zone 1-	The maximum height permitted is 120 feet.
Building Zone 2-	The maximum height permitted is 140 feet.
Building Zone 3-	The maximum height permitted is 160 feet.

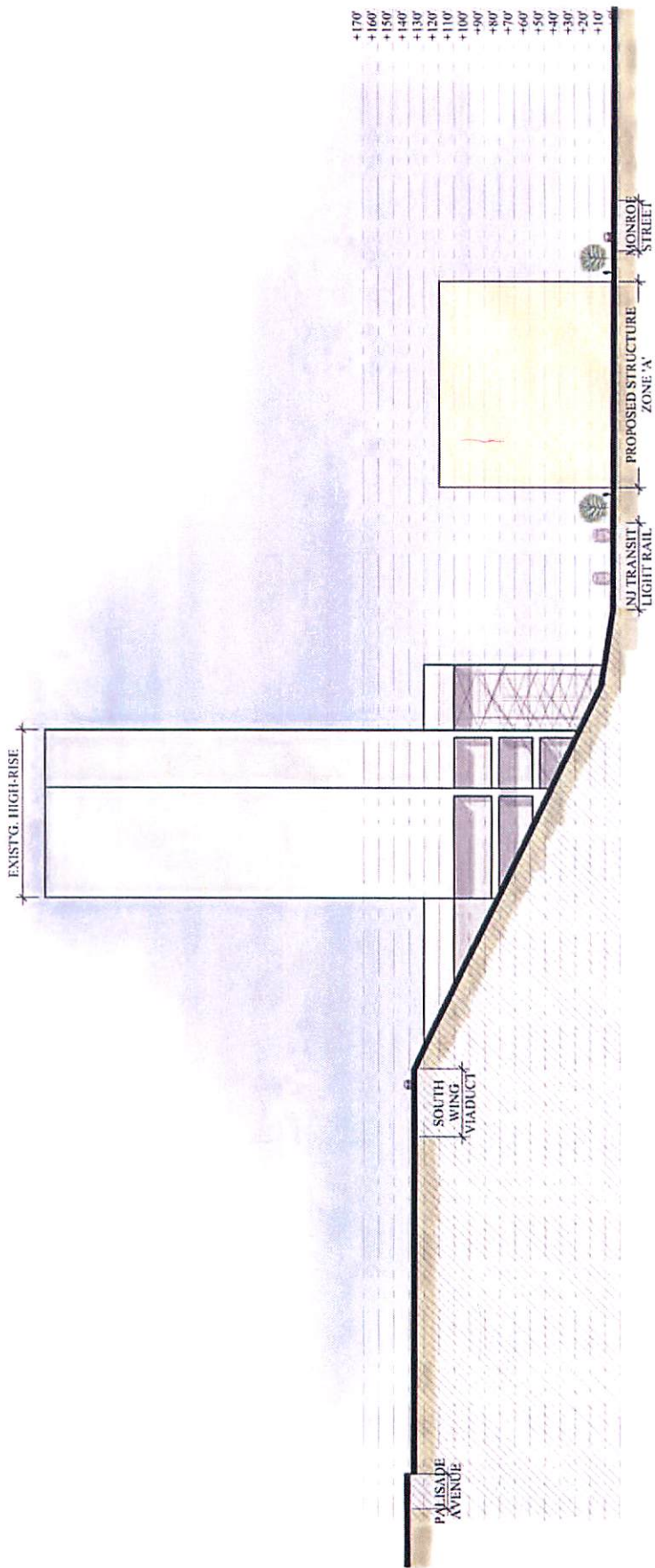
- Rooftop appurtenances may cover no more than twenty (20) percent of the roof area, with height limited to 18 feet above the roof slab. Every effort shall be made to cluster the appurtenances and to screen them from view. The materials used for screening purposes shall not be counted toward roof coverage. Coverage created as a result of "green" technology installations (e.g., vegetation, solar panels, etc.) shall be exempt from the overall coverage calculation.

Building Setbacks in All Building Zones

Setbacks for façades are measured relative to the property line and may refer only to the first level or to all levels. Stepbacks, in contrast, are measured with respect to lower façades and refer to upper level street-facing façades.

- Along all public streets a building setback of a minimum of three (3) feet to a maximum of ten (10) feet shall be provided.
- Along the HBLR tracks buildings should be set back at least forty (40) feet to allow for the creation of the green circuit.
- Adjacent to the 14th Street viaduct a minimum building setback of fifty (50) feet shall be provided.
- Setback areas shall be attractively landscaped with appropriate plantings. Low fences, walls, and gates characteristic of Hoboken may be provided along the front of the setback area, not to exceed three (3) feet in height. Front stoops leading to a residence may also be in the setback.

² According to the Hoboken zoning, building height is measured from the average grade computed by averaging the grade obtained at the four (4) corners of a principal structure or the four (4) most extreme points on the north, south, east and west sides of a principal structure, or at four (4) points ninety degrees (90°) apart for a circular structure.



SECTION 1
1"=80'

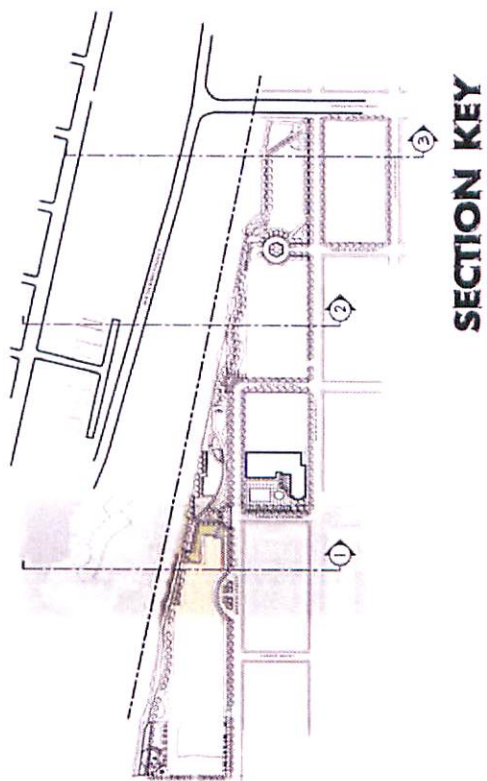


Figure 11: Permitted Building Heights in Building Zone 1

- Where a major building entry or a retail space is located, a plaza area with deeper setbacks is permitted. Such plazas shall not exceed fifty (50) feet in any one dimension. In order to maintain activity in the space, plazas must be located adjacent to building entries.

Building Stepbacks in Building Zone 3

Stepbacks refer to upper level façades as measured with respect to lower façades.

- Along the Jefferson Street frontage, façade areas above 60 feet from the grade of the sidewalk shall have at least one stepback of a minimum of five (5) feet with respect to lower levels in order to create a “datum” line that relates to the prevailing mid-rise scale of Hoboken by emphasizing an equivalent five-story height. (Note that these upper floor stepbacks shall be measured with respect to the lower stories not to the street-facing property line. The minimum setback relative to the street is thus 8 feet.) Acceptable stepback methods are a flat stepback or a pitch with 1:1 or shallower slope. There is no maximum stepback for façade areas above 60 feet.

Separation between Buildings Bays and Wings in All Building Zones

- A minimum separation of eighty feet (80') shall be provided between facing facades of residential structures higher than sixty feet (60') within each building zone. In addition, the average separation between facing facades of residential structures higher than sixty feet (60') across all building zones must be at least one hundred and twenty feet (120').

Building Height Locations in All Building Zones

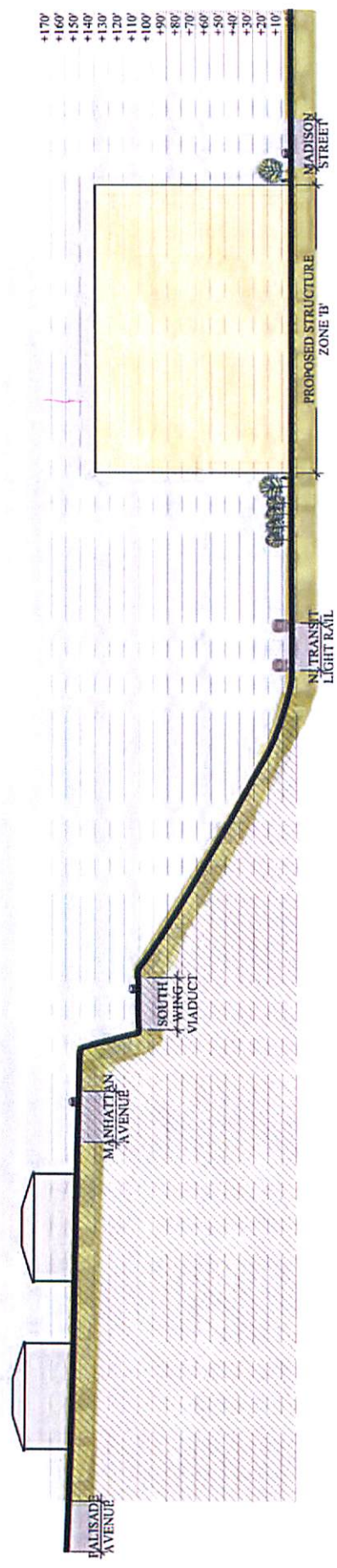
- Portions of buildings taller than five (5) stories or sixty feet (60') shall be oriented east to west (i.e., the longer dimension on an east to west axis) so as to reduce mass, preserve views through to the Palisades and also maximize views of project residents.

Parking and Loading Requirements

Parking Ratios in All Building Zones

Residential Parking Ratios

- The minimum off-street parking ratio for residential uses in the 900 Monroe Building Zone is 0.75 space per unit. The maximum parking ratio is 1.2 spaces per unit.
- The minimum off-street parking ratio for residential uses in Building Zones 1, 2 and 3 is 0.75 spaces per unit. The maximum parking ratio is 1.0 spaces per unit.



SECTION 2
1"=80'

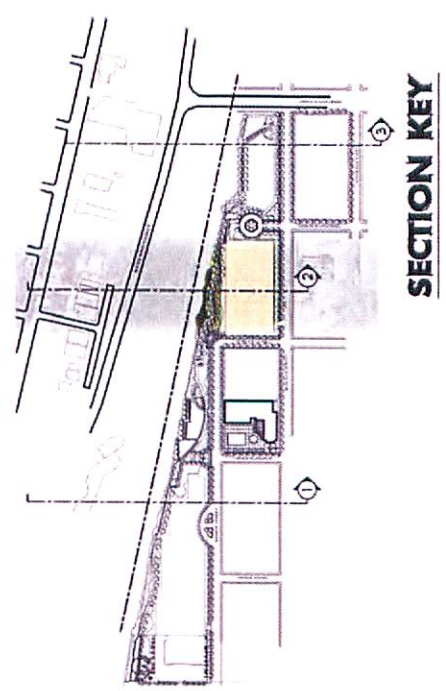


Figure 12: Permitted Building Heights in Building Zone 2

Non-Residential Parking Ratios

- In the 900 Monroe Building Zone, non-residential uses shall provide off-street parking at the minimum rate of 2.5 spaces per 1,000 square feet of non-residential floor space. The maximum parking rate shall be 2.8 spaces per 1,000 square feet of non-residential floor space.
- If the non-residential uses in Building Zones 1, 2 and 3 total less than 30,000 square feet, then no off-street parking is required. All parking for non-residential uses up to 30,000 square feet should be provided on-street. Any increment of non-residential uses in these three building zones exceeding 30,000 square feet shall provide parking off-street at the rate of 2.5 spaces per 1,000 square feet of non-residential floor space.

Parking for Parks, Open Space and Recreation

Since Hoboken is a walkable, compact community, and the Redevelopment Area is served by the HBLR, no additional parking is required for parks, open spaces and recreation.

Garage Entries

- Each garage entry and curb cut shall not exceed twenty (20) feet in width.
- Garage entries shall have remotely-operable automatic doors which remain closed when not in use. Garage doors and entries shall be architecturally detailed in a similar manner to the surrounding façade design, including glazing types and patterns.

Parking Location and Screening

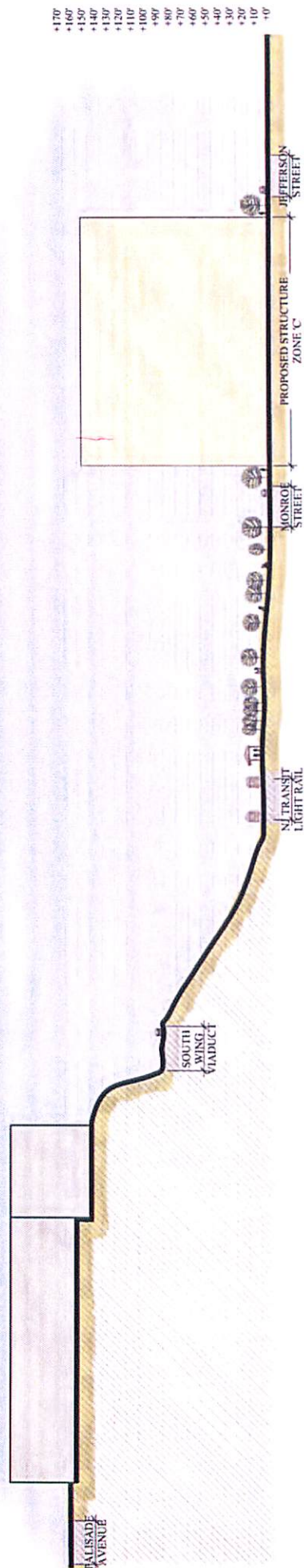
- At-grade or above-grade parking should be screened from view of streets, pathways, parks, and other open spaces by active uses or architectural screening. In this way, the visual intrusion of parking is minimized and the building maintains an active relationship to the street. Further regulations for off-street parking are described in the section on *Screening of Above-Grade Parking*.

Loading

- Requirements for loading for residential or non-residential uses shall be determined during site plan review.

Bicycle Storage

- Each building shall provide secure a lockable storage room for residents' bicycles within the building.



SECTION 3
1"=80'

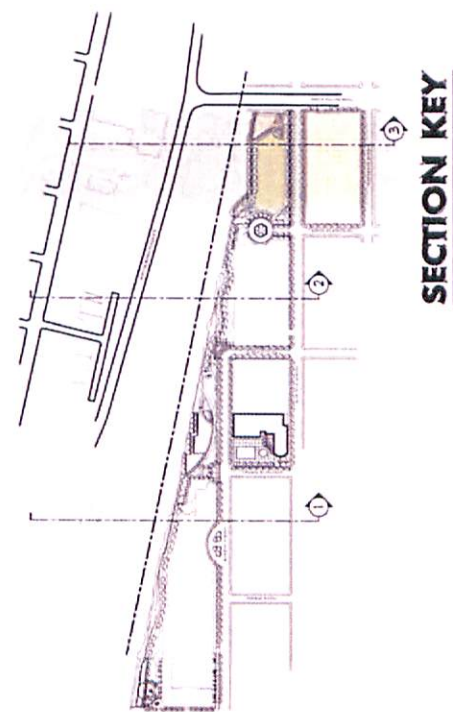


Figure 13: Permitted Building Heights in Building Zone 3

5



Site Planning & Design

Intent

This section sets forth standards for the Redevelopment Area that are designed to foster a walkable and pedestrian-scaled neighborhood with visually interesting and high-quality buildings that complement the network of streets, parks and other public spaces. The design standards seek to create a “sense of place” by emphasizing important visual corridors; by shaping buildings to create an active relationship with streets, the green circuit and parks; and by screening parking or locating it within buildings and within the interior of blocks.

Screening of Above-Grade Parking

Facing Streets and Park Blocks

- All off-street parking areas that face streets, or park blocks shall be screened from view by primary or secondary permitted uses. Refer to **Figure 14: Screening of Above Grade Along Street Facing Frontage**.

Facing HBLR Right-of-Way and 14th Street Viaduct

- Parking areas may be located behind primary or secondary active uses, or may be architecturally screened according to the following requirements:
 - The façades of exposed parking areas shall be articulated and broken down in scale to create a more varied and textured appearance, with window-like openings and architectural detailing. Monotonous ribbon or banded-style parking façades, whether open or glazed, are prohibited.
 - The portion of the façade that screens the parking area shall be broken into 20- to 50-foot structural bays, in the same manner as described in **Building Massing** below.
 - The lower level(s) of above-grade parking shall be designed to “read” as a base to the building, using, for example, heavier-appearance

materials, materials that contrast with those used at the middle and top, and more elaborate architectural detailing befitting a pedestrian-scale base.

- The parking façade shall use a similar or complementary type and quality of materials as the residential portion above.
- Large areas of blank, solid walls are prohibited along street-facing parking areas.
- Window openings are required. Window glazing should be translucent or transparent; clear-tinted or reflective glazing is prohibited. Decorative metal grilles may be used in combination with glazing on garage openings. The size, spacing, and enframement of garage window openings should be similar to that of adjacent retail and/or residential windows. Furthermore, the pattern of garage window openings shall align vertically with the pattern and/or spacing of residential bays and windows above.
- The pattern of windows and detailing in parking façades shall disguise any internal ramps or sloping levels of the parking garage so that they appear level.
- The façades of any exposed parking structures shall include a minimum three (3) foot deep landscaped area to soften the appearance of the parking garage.

Screening of Back Room Uses and Storage Facilities

Back Room Uses

- Any “back room” facilities (as defined in the earlier section on Accessory Uses) on any level shall be screened from view of public streets, using the same methods described above for *Screening of Above-Grade Parking*.
- Trash and recycling bins, sheds, and storage areas shall be enclosed at the sides and top and located either indoors or outdoors at a minimum of 10 feet from any street or property line.

Storage Facilities

- Any storage lockers and similar facilities provided for the use of building residents shall not be located adjacent to exterior façades, because boxes and goods stored within could obstruct building windows. Instead, storage lockers and similar facilities may be located at the interior of the building, accessed by a hallway that abuts the exterior façade. In this way, windows onto the hallway may be provided as a means of architectural screening to disguise the storage area. There is no limit to the amount or length of residential storage lockers provided, as long as they are located in this manner.



Figure 14: Screening of Above Grade Parking Along Street Facing Frontage

Building Massing

Horizontal Differentiation

Horizontal Length

- Portions of buildings above 5 stories or 60 feet in height shall not extend horizontally more than 70 feet in length measured parallel to Monroe, Madison and Jefferson Streets, in order to preserve views of the Palisades.

Bays

All building façades shall be broken into 20 to 50 foot-wide vertical bays.

- Bays shall be defined through elements such as columns, pilasters, gutters or expansion joints, changes in façade plane, size and rhythm of window spacing, pattern of balconies, and/or variation in surface material and pattern.
- Bay definition should extend through all levels of the building(s). Exceptions are permitted where horizontal massing changes in the façade plane or expression lines are provided to break up the building bulk.

- The spacing and pattern of any retail storefront or other non-residential façade areas shall relate to the building's vertical bay pattern.

Corner Treatments

- To help mitigate the visual bulk at the tallest portions of building(s) architectural variety should be employed at corners. This could include fully-glazed corner windows or bays, stepbacks and height reductions at corners, and variation in materials.

Balconies and Terraces

- Shallow or "Juliet" balconies (or balustrades) are permitted. Juliet balconies shall be of open metalwork, connected to a fully-functional balcony door, and shall be detailed to match any other true, recessed balconies in the same façade.
- Balconies over two (2) feet in depth (measured perpendicular to the façade plane) are discouraged facing public streets; if provided they shall be entirely recessed within the primary façade plane.
- Terraces are permitted and encouraged within required stepbacks along Jefferson Street frontages and within any other optional stepback areas. Terrace railings and walls shall be an integral part of the building façade design.
- Balconies are encouraged for interior-facing units of the building.

Vertical Differentiation

In order to minimize the impacts of building bulk and height, building façades adjacent to streets shall provide for a differentiated building mass consisting of a base, middle, and top, as follows:

Base

- The base is the lowest one or two levels of the building.
- The base of the building shall be more richly-detailed and articulated than upper levels, in order to relate to pedestrians on the sidewalk.
- The base of the building should be highlighted with a variation in architectural treatment compared to the upper levels. For example, in more traditional designs, the base of the building should appear heavier than the upper levels, to visually ground the building. For more modern designs, variation in glazing pattern, materials, and floor heights may help define the base. Retail signage, building-mounted lighting and awnings or canopies should also be used to help highlight the base.

Middle

- The middle of the building shall be distinguished from the base and top by horizontal belt courses or cornices; and/or changes in material, masonry or fenestration pattern.

Top

- The top of a building shall be defined and differentiated at the topmost floor(s) and the roofline.
 - The roofline shall be emphasized with a parapet wall and/or balustrade or deep cornice with a minimum 8-inch projection.
 - The upper levels of the building—those higher than 60 feet—shall be designed to be visually lighter than the lower floors, such as through a lighter color material or a larger proportion of glazing.

Building Transparency

Building Entries and Porches

Residential Entries

Stoops and lobbies are required in order to create a transition from the private space of a residence to streets and other public spaces and create a “friendly” appearance. Requirements for stoops and lobbies are as follows:

- In lower-scale building masses (less than sixty feet in height), an individual entrance with a front stoop shall be provided every fifty (50) feet for the first-level residential units.
- Taller-scale building masses (greater than sixty feet in height) shall have a shared entry and lobby, highlighted by building massing, glazing, and/or an overhang. Shared residential entries leading to lobbies and elevators shall be located directly along and facing the street, or may be located on a major courtyard with frontage on Monroe, Madison or Jefferson Streets. Individual unit entries are also encouraged for any first-floor residential units within taller-scale buildings.
- Ancillary entrances are encouraged along the green circuit and other park and open space areas.

Non-Residential Entries

- Each of any non-residential uses (i.e., retail or other secondary uses) shall have its own individual entrance from the sidewalk. Mall-style interior entrances are prohibited for these uses.
- For a large retail tenant space, at least one entrance should be provided for every increment of 50 linear feet of street frontage. This will provide multiple connections to the sidewalk and allow for future flexibility in case the large space is retrofitted for several small businesses in the future.

Windows

Window size and proportion are allowed to vary in order to complement a variety of building styles. The minimum percent of the façade area of each level of buildings devoted to windows shall be as follows.

First Level Windows

- For secondary permitted uses on the first level, storefront-style plate glass windows shall be provided, occupying at least fifty (50) percent of the façade area.

- For residential uses on the first level and for parking areas disguised with architectural detailing on the first level, windows shall be provided, occupying at least twenty-five (25) percent of the façade area.

Upper-Level Windows

- Above the first level, windows shall occupy at least twenty-five (25) percent of the façade area, regardless of the uses at that level.

Materials

The following material requirements apply to all façades.

- Stone, masonry, brick, wood, and glass, as well as cast iron, steel, aluminum and other types of metal are preferred primary materials for façades. However, no more than three different materials should be used as primary materials on the building façade. Within the primary materials, variations in colors, textures, and pattern may be employed to further break up the building bulk.
- Security gates, access panels, and garage windows grilles should be enlivened with artwork, decorative tiling or ornamental metalwork where possible.
- Materials should be extended around corners and extensions in order to avoid a “pasted-on” appearance. All building façades adjacent to or easily visible from a public street or park block should exhibit the same degree of architectural detailing as the building’s adjacent façade.
- Reflective, mirrored, smoked, and dark tinted glass is prohibited. Lightly-tinted glass is permitted for energy-efficiency reasons only if the lighter-color tint that is closest to clear is used. Translucent glass, which allows for shadows and silhouettes behind the glass, should be used rather than spandrel glass, which is virtually opaque, for areas where light is to be admitted while direct views are obscured (such as screened parking areas or mechanical rooms.)

Green Design

Rooftop Decks and Landscaping

Rooftop gardens and landscaped areas provide private open space for new residents (thereby reducing the demand for public open space), and serve to reduce stormwater runoff (which can help address flooding concerns in the area). Green roofs also improve urban air quality and cut energy costs. These private open spaces enhance the value of the development and transform areas that would otherwise be unattractive and unusable into aesthetically pleasing and usable open spaces.

Each rooftop level, including courtyards over garages, shall be designed to meet one of the following two requirements:

- Be designed to be LEED-qualified. LEED goals for roofs include reducing the heat island effect and minimizing impervious surfaces.

- Provide resident access and landscaping as follows:
 - Between 25 and 50 percent of the rooftop area shall be devoted to deck areas accessible to all building residents;
 - The remainder of the roof area shall be attractively landscaped to provide a visual amenity and to provide shade and insulation for the building below.

Refer to **Figure 15: Concept Drawing of Rooftop Garden**

LEED Certification

All buildings in the Redevelopment Area shall achieve certification under the Leadership in Energy and Environmental Design (LEED) rating system for green building design, sponsored by the US Green Building Council (USGBC). Broadly speaking, green building design goals include reduced energy and water use; use of sustainable, renewable, non-toxic and locally-produced materials; improved indoor air quality; environmentally-conscious site planning; and a location in a central, developed area with existing services and resources. The USGBC has developed a variety of standards for evaluating green building design in several categories, including LEED-NC (for New Construction), and is preparing pilot program guidelines for multi-family residential developments (LEED-ND for Neighborhood Development). Specifically, buildings shall achieve a certified or higher status; that is, a minimum score of 26 out of a total possible 69 points under the LEED-NC system, or a minimum score of 40 out of a total possible 106 points under the LEED-ND system. (LEED-ND is currently in pilot testing until late 2009, when it is expected to become available for public use.)

Site Design

Site design shall seek to minimize environmental damage and reduce energy use and strain on municipal utilities and impact on adjacent uses by means of the following measures.

Stormwater Management

- Development shall provide features to reduce stormwater runoff rates consistent with existing regulations promulgated by the New Jersey Department of Environmental Protection (NJDEP).

Outdoor Lighting

- All outdoor lighting, including street lamps and accent lighting, shall comply with "dark sky" standards intended to reduce light pollution. Dark sky standards require that lighting is downcast, illuminates only the intended areas, and does not cause disabling glare that affects driver safety and reduces the visibility of starry night skies. Additional information on dark sky goals and regulations may be found at the International Dark-Sky Association's web site, www.darksky.org.
- Bright, stadium-style lighting is prohibited, including for park areas.

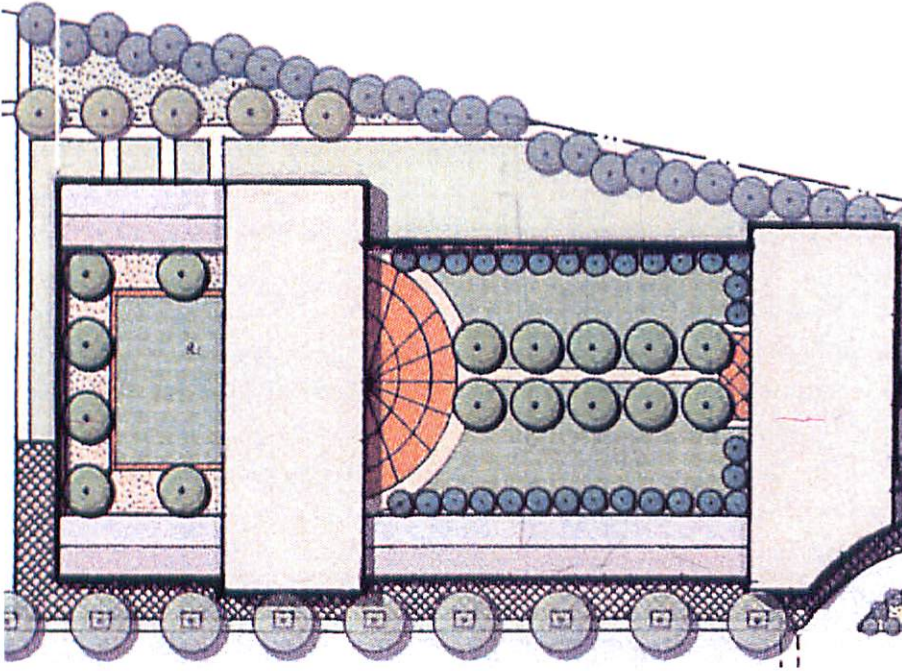


Figure 15: Concept Drawing of Rooftop Garden

Signage

The following regulations apply to signage for any non-residential (secondary) uses:

- Building design shall provide a logical space within the first level, above any retail or commercial storefronts, doors, or windows, for retail signage. Signage shall be placed so as not to obscure the structural or architectural elements of the building.
- Building mounted signs shall only display information about the businesses located on the property.
- Internally illuminated signs shall not have a white or light-colored background or signboard.
- Sign illumination shall not cast light into residential windows.
- Freestanding signs are prohibited.

Other regulations pertaining to signs shall be pursuant to Article IX of the City of Hoboken Zoning Ordinance, as specifically provided for the City's residential zones.

6

Plan Consistency Review



Relationship to Hoboken Zoning Code

This Redevelopment Plan shall supersede all provisions of the Zoning and Development Regulations of the City of Hoboken regulating development in the area addressed by this Redevelopment Plan. In all situations where zoning issues are not specifically addressed herein, the Hoboken Zoning and Development Regulations shall, however, remain in effect. Final adoption of this Plan by the City Council shall be considered an amendment of the City of Hoboken Zoning Map.

Relationship to Adjacent Municipalities

The Redevelopment Area is situated in the northwestern section of the City of Hoboken. The Redevelopment Area is adjacent to the City of Union City and in proximity to the City of Weehawken.

Union City

Relationship to Union City Master Plan

Union City borders the northwest corner of Hoboken along the Palisades, a portion of which is adjacent to the Redevelopment Area. The Land Use Element of the Master Plan for the City of Union City was adopted in 1975 and is somewhat outdated. However, the Land Use Plan states that Union City is an urbanized Metropolitan City that is largely built out. The Plan puts forth several planning objectives which seek to emphasize residential land uses and particularly supports increased residential development along the Palisades.

A Master Plan Reexamination report was prepared in 2000 by the City of Union City. It recommends that the City revise its steep slope ordinance to allow for additional development on the Palisades while protecting the Palisade cliffs. The report also recommends that the parcels on top of the Palisades be located in the multi-family residential zone to allow for the provision of more housing in the southern portion of the City. These recommendations are generally consistent

with the Western Edge Redevelopment Plan.

Weehawken

Relationship to Weehawken Master Plan

Hoboken shares its northern boundary with the Township of Weehawken. The Land Use Plan of the Weehawken Master Plan was adopted in 1976 and was amended in 1984. A reexamination report was adopted in 1991. The "Shades" section of Weehawken adjacent to Hoboken is designated for Medium Density Residential use. The area adjacent to the Shades is currently zoned I-2. The Hoboken Master Plan recommends that the City rezone this area so that it is consistent with the residential zoning in Weehawken. The land use recommendations in the Weehawken Master Plan are generally consistent with the Western Edge Redevelopment Plan.

Relationship to the Hudson County Master Plan

The Hudson County Master Plan was adopted in February 2002. The General Goals of the plan are as follows:

1. To improve the overall quality of life in Hudson County.
2. To provide for the economic revitalization of the County's commercial and industrial base.
3. To preserve the character of existing well-established neighborhoods.
4. To improve the transportation network.
5. To increase the tax base.

The overall objectives and policies of the Western Edge Redevelopment Plan are consistent with these goals.

Relationship to State Development and Redevelopment Plan

Among the State Plan's intentions is to revitalize the state's existing urban areas by directing growth and development to those areas. On the State Plan Policy Map, the Redevelopment Area is located in the Metropolitan Planning Area, which is identified in the State Plan as an appropriate location for much of the State's new growth.

A stated goal of the State Plan is to revitalize the State's cities and towns by protecting, preserving and developing the valuable human and economic assets in cities, towns and other urban areas. The Redevelopment Area, by virtue of its location within a part of New Jersey that has extensive existing infrastructure and a long history of development, is by all measures an appropriate location for growth and redevelopment. The Redevelopment Plan will facilitate growth in this area and contribute to the economic revitalization of the State. The objectives of the Western Edge Redevelopment Plan are consistent with the goals, strategies and policies of the State Plan. The Redevelopment Plan will redevelop a deteriorated and underutilized area within Hoboken, placing currently unproductive lands into productive use in order to better serve the needs of residents and the City as a whole.

7



Redevelopment Actions

Outline of Proposed Actions

I. Demolition

It is proposed that the Plan Area be completely cleared of existing structures.

II. New Construction

Construction of new structures and other improvements will take place as proposed in Chapters 3, 4, and 5 of this Redevelopment Plan. The redeveloper must adhere to the overall parameters for development presented in Chapters 3, 4 and 5 of this Plan and is encouraged to otherwise refine the design concepts presented therein in developing a unique and high-quality project proposal. Once a redeveloper is selected, the redeveloper will be required to enter into a Redeveloper's Agreement with the City that stipulates the precise nature and extent of the improvements to be made and their timing and phasing as permitted therein.

Properties to be Acquired

The Redevelopment Plan does not anticipate the need to acquire privately-owned property within the Plan Area. However, the City reserves the right, up until such time as the redevelopment within the Plan Area is deemed complete, to acquire any or all of the privately-owned lots in the designated Redevelopment Area should it be necessary to facilitate redevelopment.

Relocation

The City does not anticipate the displacement or relocation of any residents or businesses within the Plan Area.

Other Actions

In addition to the demolition, new construction and acquisition described above,

several other actions may be taken by the governing body to further the goals of this Plan. These actions may include, but shall not be limited to: (1) provisions for public infrastructure necessary to service new development, (2) environmental remediation, (3) vacation of public utility easements and other easements and rights-of-way as may be necessary for redevelopment.

8

General Provisions



Definitions

Except for those words defined below in this Section, words that appear in this Redevelopment Plan shall be defined in accordance with the definitions that appear in the City's land use ordinances, or, where these ordinances do not provide a definition, in accordance with the definitions in the Municipal Land Use Law.

Easements

No building shall be constructed over a public easement in the Redevelopment Area without prior written approval of the City of Hoboken.

Site Plan and Subdivision Review

Prior to commencement of construction, site plans for the construction and/or rehabilitation of improvements within the Redevelopment Area, prepared in accordance with the requirements of the Municipal Land Use Law (N.J.S.A. 40:55D-1 et seq.), shall be submitted by the applicants for review and approval by the Hoboken Planning Board.

Any subdivision of lots and parcels of land within the Redevelopment Area shall be in accordance with the requirements of this Redevelopment Plan and the subdivision ordinance of the City of Hoboken, except that where this redevelopment plan contains provisions that differ from those in the subdivision ordinance, this plan shall prevail.

Approvals by Other Agencies

The redeveloper shall be required to provide the City with copies of all permit applications made to federal, state and county agencies upon filing such applica-

tions, as will be required by the redeveloper's agreement to be executed between the redeveloper and the City.

Adverse Influences

No use or reuse shall be permitted which, when conducted under proper and adequate conditions and safeguards, will produce corrosive, toxic or noxious fumes, glare, electromagnetic disturbance, radiation, smoke, cinders, odors, dust or waste, undue noise or vibration, or other objectionable features so as to be detrimental to the public health, safety or general welfare.

Non-Discrimination Provisions

No covenant, lease, conveyance or other instrument shall be affected or executed by the City Council or by a developer or any of his successors or assignees, whereby land within the Redevelopment Area is restricted by the City Council, or the developer, upon the basis of race, creed, color, or national origin in the sale, lease, use or occupancy thereof. Appropriate covenants, running with the land forever, will prohibit such restrictions and shall be included in the disposition instruments. There shall be no restrictions of occupancy or use of any part of the Redevelopment Area on the basis of race, creed, color or national origin.

Duration of the Plan

The provisions of this Plan specifying the redevelopment of the Redevelopment Area and the requirements and restrictions with respect thereto shall be in effect for a period of 40 years from the date of approval of this plan by the City Council.

Deviation Requests

The Hoboken Planning Board may grant deviations from the regulations contained within this Redevelopment Plan where, by reason of exceptional narrowness, shallowness or shape of a specific piece of property, or by reason of exceptional topographic conditions or physical features uniquely affecting a specific piece of property, the strict application of any area, yard, bulk or design objective or regulation adopted pursuant to this Redevelopment Plan would result in peculiar practical difficulties to, or exceptional and undue hardship upon, the developer of such property. The Hoboken Planning Board may also grant such relief in an application relating to a specific piece of property where the purposes of this Redevelopment Plan would be advanced by a deviation from the strict requirements of this Plan and the benefits of the deviation would outweigh any detriments. No relief may be granted under the terms of this section unless such deviation or relief can be granted without substantial detriment to the public good and without substantial impairment of the intent and purpose of the Redevelopment

Plan. An application for a deviation from the requirements of this Redevelopment Plan shall provide public notice of such application in accord with the requirements of public notice as set forth in N.J.S.A. 40:55D-12.a. and b.

Notwithstanding the above, any changes to the uses permitted in the Redevelopment Area shall be permitted only by means of an amendment of the Redevelopment Plan by the governing body, and only upon a finding that such amendment would be consistent with and in furtherance of the goals and objectives of this Plan.

Escrows

The redeveloper shall be responsible to post sufficient escrows to cover any and all costs of the professional consultants retained by the City to review the proposed redevelopment project and advise the City on any and all aspects of the redevelopment process.

Infrastructure

The redeveloper, at the redeveloper's cost and expense, shall provide all necessary engineering studies for, and construct or install all on- and off-site municipal infrastructure improvements and capacity enhancements or upgrades required in connection with the provision of water, sanitary sewer, and stormwater sewer service to the project, in addition to all required tie-in or connection fees. The redeveloper shall also be responsible for providing, at the redeveloper's cost and expense, all sidewalks, curbs, streetscape improvements (street trees and other landscaping), street lighting, and on- and off-site traffic controls and road improvements for the project or required as a result of the impacts of the project. The Redevelopment Agreement between the City and the redeveloper will contain the terms, conditions, specifications, and a description of required performance guarantees (such as performance bonds or other acceptable performance security) pertaining to redeveloper's obligation to provide the infrastructure and improvements required for the project.

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Other Provisions

In accordance with N.J.S.A. 40A:12A-1 et seq., known as The Local Redevelopment and Housing Law, the following statements are made:

- The Redevelopment Plan herein has delineated a definite relationship to local objectives as to appropriate land uses, density of population, and improved traffic and public transportation, public utilities, recreation and community facilities and other public improvements. The Plan has laid out various programs and strategies needed to be implemented in order to carry out the objectives of this Plan.
- The Redevelopment Plan lays out the proposed land uses and building requirements for the Redevelopment Area.
- The Redevelopment Plan does not envision a need to acquire privately-owned properties or to relocate any residents or businesses.
- The Redevelopment Plan is substantially consistent with the Master Plan for the City of Hoboken. The Plan also complies with the goals and objectives of the New Jersey State Development and Redevelopment Plan.
- This Redevelopment Plan shall supersede all provisions of the Zoning and Development Regulations of the City of Hoboken regulating development in the area addressed by this Redevelopment Plan, except where specifically mentioned within the text of this Plan. In all situations where zoning issues are not specifically addressed herein, the Hoboken Zoning and Development Regulations shall, however, remain in effect. Final adoption of this Plan by the City Committee shall be considered an amendment of the City of Hoboken Zoning Map.
- If any section, paragraph, division, subdivision, clause or provision of this Redevelopment Plan shall be adjudged by the courts to be invalid, such adjudication shall only apply to the section, paragraph, division, subdivision, clause or provision so judged, and the remainder of this Redevelopment Plan shall be deemed valid and effective.

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Procedure for Amending the Approved Plan

This Redevelopment Plan may be amended from time to time upon compliance with the requirements of state law. A non-refundable application fee of \$5,000 shall be paid by the party requesting such amendment, unless the request is issued from an agency of the City. The City of Hoboken Council, at its sole discretion, may require the party requesting the amendments to prepare a study of the impact of such amendments, which study must be prepared by a professional planner licensed in the State of New Jersey.